

1990

Moody 35





Moody 35

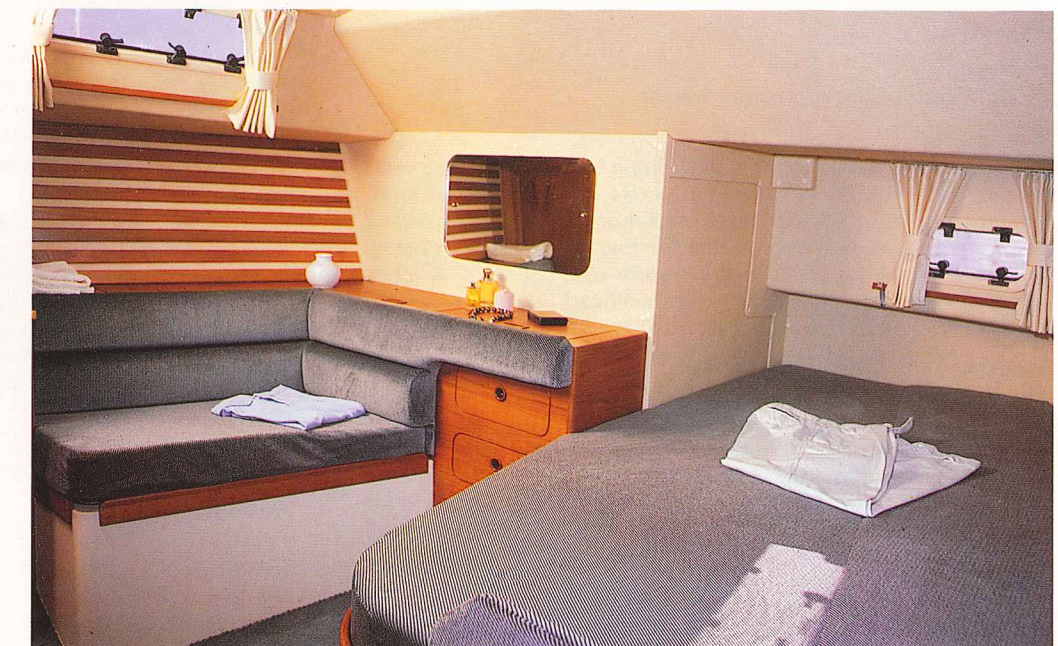
A new centre cockpit yacht, designed by Bill Dixon, which continues the Moody development of craft of sleek, modern design, superb cruising performance and offering exceptional seagoing comfort for the family.

With the beautifully appointed, light and spacious accommodation, big cockpit, after deck seating and transom bathing platform providing everything for relaxed living at sea.

The 35 represents the latest development in performance cruising yacht concept whilst retaining the traditional Moody standards of excellence. A safe, fast craft that is easy to handle and highly seaworthy.

The new Moody 35 – performance cruising in style.

With clear deck areas, spacious and safe cockpit, transom bathing platform, and after deck seating, the 35 is designed to provide the maximum pleasure for the family at sea.



OWNER'S AFT STATEROOM

Complete with full sized double berth, settee, wardrobe and en-suite access to the toilet compartment.



NAVIGATION AREA

Situated aft of the saloon on the port side, with teak desk containing ample chart stowage space.



TOILET COMPARTMENT

With marine toilet, wash basin, hot and cold water system and separate shower area.



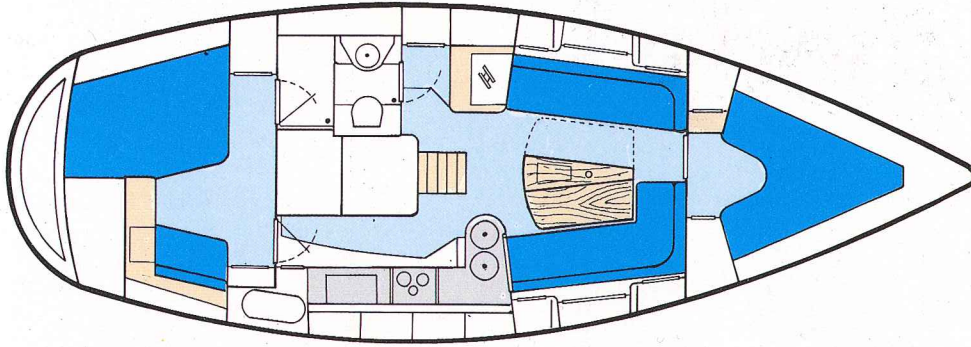
SALOON

The light, airy saloon with L shaped settee to starboard, single settee to port and large folding leaf table.

GALLEY

The galley, aft of the saloon to starboard, has a two burner cooker, twin sinks, refrigerated ice box and ample work surfaces and storage space.





Principal Dimensions:	LOA	34'6"	10.52m
	LWL	28'10"	8.78m
	Beam	11'10½"	3.62m
	Draft: Fin	5'3"	1.60m
	Bilge	3'11"	1.19m
	Fuel Capacity	41 gallons	186 litres
	Water Capacity	43 gallons	195 litres
Displacement:	Fin	12800lbs	5798kg
	Bilge	13300lbs	6025kg
Ballast:	Fin	4750lbs	2152kg
	Bilge	5250lbs	2378kg
Rig	I	42'3"	12.88m
Dimensions:	J	13'6"	4.09m
	P	35'9"	10.90m
	E	12'6"	3.78m
Sail Areas:	Mainsail	247sq ft	22.95sq m
	Working jib	258sq ft	23.97sq m

Deck and Hull

In glass reinforced plastic to Lloyds approved specification, each yacht carries a Lloyds hull construction certificate. The hull is white and the deck is white with grey non slip surfaces. The keel is cast iron and externally bolted on. Underwater sections are antifouled, the hull has style lines and boot top and deck style lines.

Fore peak

Chain locker with access from foc'sle.

Fore cabin

A comfortable roomy cabin with 2 single berths which are convertible to a double. Under the berths are generous storage spaces complete with PVC liner bags. Locker space is provided along the ship's sides above the berths. A hanging locker is provided aft of the berth with a vanity unit opposite. A large opening deck hatch is fitted in the deckhead for light and ventilation. The cabin is fitted in teak with carpeted floor surfaces.

Saloon

The light airy saloon has an L shaped settee to starboard and a single settee opposite to port, a large folding leaf dining table is centrally mounted. Below the settees are rigid water tanks. Storage space is provided outboard of settee backs and in locker and shelving units above. The saloon is fitted out in teak with choice of soft furnishings and carpets. Ventilation is provided by an opening deck hatch and ventilators.

Navigation area

Is situated aft of the saloon on the port side. The chart table is a teak desk with an opening top to provide chart storage space. Locker space is provided below the chart table. Console space is provided for instruments and repeaters and the electrical distribution board complete with circuit breakers and indicator lights is fitted adjacent to the table. Aft of the chart table is the oilskin hanging locker.

Toilet compartment

Aft of the navigation area is on the port side. This compartment is fully fitted out with a marine toilet, hot and cold pressurized water, wash basin and separate shower area. Shower waste water is removed by an electric pump. Ventilation and light are provided by an opening portlight. En-suite access is also provided to the aft cabin.

Galley

Situated aft of the saloon on the starboard side, this area is fully fitted out with pressurized hot and cold water to twin sinks, one with chopping board cover, 2 burner cooker with oven and grill fitted in gimbals with stainless steel crash bar and fold away worktop. A top opening refrigerated ice box is fitted below the galley worktop. There is ample storage space with lockers and drawers for crockery and cutlery. The galley is fitted out in teak with easily cleaned laminate worktops.

This is the outline specification of the Moody 35 and is intended to be a fair representation. However the right to add to, omit from or amend this specification without prior notice is reserved. (This specification does not constitute an offer to contract).

Aft cabin

Is reached through the passageway from the saloon and has en-suite access to the toilet compartment. This cabin is remarkably spacious with a full size double berth, settee, wardrobe and locker space provided. Light and ventilation are provided by opening portlights and deck hatch.

Cockpit

The large centre cockpit has seating on 3 sides with well designed coamings for comfort and security. Wheel steering is fitted with binnacle guard, compass and engine controls. The engine instrument panel is set into the coamings and a sailing instrument console fitted over the main hatchway complete with speedo/log and echo sounder. All sail controls are led aft to the cockpit. Cockpit sole and sides are teak finished.

Engine

Thornycroft T80 35hp diesel with reduction gearbox, stainless steel propeller shaft, battery state indicator, alternator charging from the engine. Master distribution panel incorporating circuit breaker switches and indicator lights. 12 volt refrigeration, full cabin lighting, navigation lights complete with deck flood light. Automatic fresh water pump, shower waste water pump, 240 volt razor socket, cathodic protection.

Instrumentation

Echo sounder, speedo/log, compass, engine instrument panel, battery state indicator, fuel and water tank gauges.

Deck fittings

Stainless steel pulpit and pushpit, complete with pushpit gate. Stanchions and sockets with twin stainless steel guard wires. Stainless steel stemhead fitted with bow roller. Four mooring cleats, 2 x 2 speed genoa sheet winches and cleats. Mainsheet complete with track and car, 2 genoa sheet tracks and cars plus turning blocks. Sheets and halliards are led aft to the cockpit with single speed halliard winch and 3 rope clutches. Handrails to coachroof, deck hatches, ventilators, opening ports. Fuel and water tank fillers, moulded in bathing platform complete with swimming ladder. Lazarette and twin deck locker storage space plus separate deck gas locker.

Spars and rigging

Silver anodised aluminium mast and boom with stainless steel standing rigging, 2 sets of aft swept spreaders. Slab reefing system including single speed winch. Running rigging comprising mainsail and fore sail halliard, main boom topping lift, kicking strap, burgee and signal halliard.

Sails

Mainsail with 3 rows of slab reefing points and a working jib supplied by a well known sailmaker complete with tacks, hanks, bags and mainsail cover.

General equipment

Compass, echo sounder with alarm, electronic speedo/log, sprayhood, anchor with 15 fathoms chain, hand operated bilge pump, 2 dry powder fire extinguishers plus automatic extinguisher in engine space, 2 warps, 2 fenders, emergency tiller, 2 winch handles, gas bottle and spanner, first aid kit, tool kit.



Moody Marketing & Development Ltd
 Swanwick Shore Road, Swanwick, Southampton SO3 7ZL, England
 Telephone: Locks Heath 885000 (UK Code 0489 UK Int. Code 44) Telex: 477536



The Sydney Morning Herald

LEISURE

No blues when on board the Moody 35

BOB WONDERS goes out in a stiff wind on Sydney Harbour in a boat that would delight even the most hardened yachting.

OK, I'll admit it, when it comes to boating, I'm a powerboat man, a revhead, a gas guzzler, call me what you will.

I prefer the acrid aroma of avgas — high-octane, aviation gasoline — to the rattle of shackles and travellers.

However, if I was going to revert to the sailboat scene (I was a yachting once), then I'd be perfectly happy aboard the Moody 35 (10.66-metre) cruising sloop.

This craft from the Moody yard in Plymouth, England, impressed me from all quarters, with its beautifully proportioned good looks, its spacious and luxurious interior and its ability to be sailed in most weather conditions with minimal crew being paramount features.

No racer, the Moody 35 nevertheless showed enough to suggest it would not be disgraced in club races, particularly if the breeze had a bit of strength on offer.

However, as a buddy of mine pointed out, the Moody 35 has no pretensions to being a racing yacht.

What we have is a comfortable passenger, offering a feeling of security from its centre cockpit arrangement and outstanding accommodation on board for a cruising couple and two or three friends.

Some yachting purists, I'm told, see the centre cockpit arrangement as "old hat", "not on" and so forth.

It was popular a few years ago, but seems to have lost a lot of appeal for whatever reason. Personally, I like it.

It offers, on a cruising boat, a good entertaining area where the crew may share the good times with a few visitors, and, it generally means the design allows for a roomy aft cabin, and ideal set-up for the cruising couple.

The aft cabin aboard the Moody 35 was superbly done. Bright and airy, thanks to port holes either side and at the transom, and a big hatch overhead, the aft cabin featured a big double berth and an opposite bench seat, all tastefully

furnished, with ample headroom, stowage and all-round comfort.

There are two means of access to this master cabin: it has its own entrance on the starboard side, adjacent to the auxiliary engine bay, or may be entered via the port side head/shower compartment, which could be adapted as an ensuite when no-one else is aboard.

Incidentally, the head is a work of art. Spacious, with a shower compartment complete with a transparent plexiglass door, vanity and excellent drainage, its moulded glass work would offer easy maintenance.

Right through the cabin areas, comprising, as well as the aft cabin, a main saloon, which included a well-planned galley and forepeak cabin, timber and joinery work presented as very craftsmanslike.

Starting forward, the forward cabin offers two berths or a filler to make up a double, with heaps of stowage beneath.

A practical touch was the inclusion under the berths of plastic liners to protect clothing and bedding stored there from becoming damp.

In the main saloon area, there are two big sofas, one to starboard is L-shaped, the other running lengthwise along the port bulkhead.

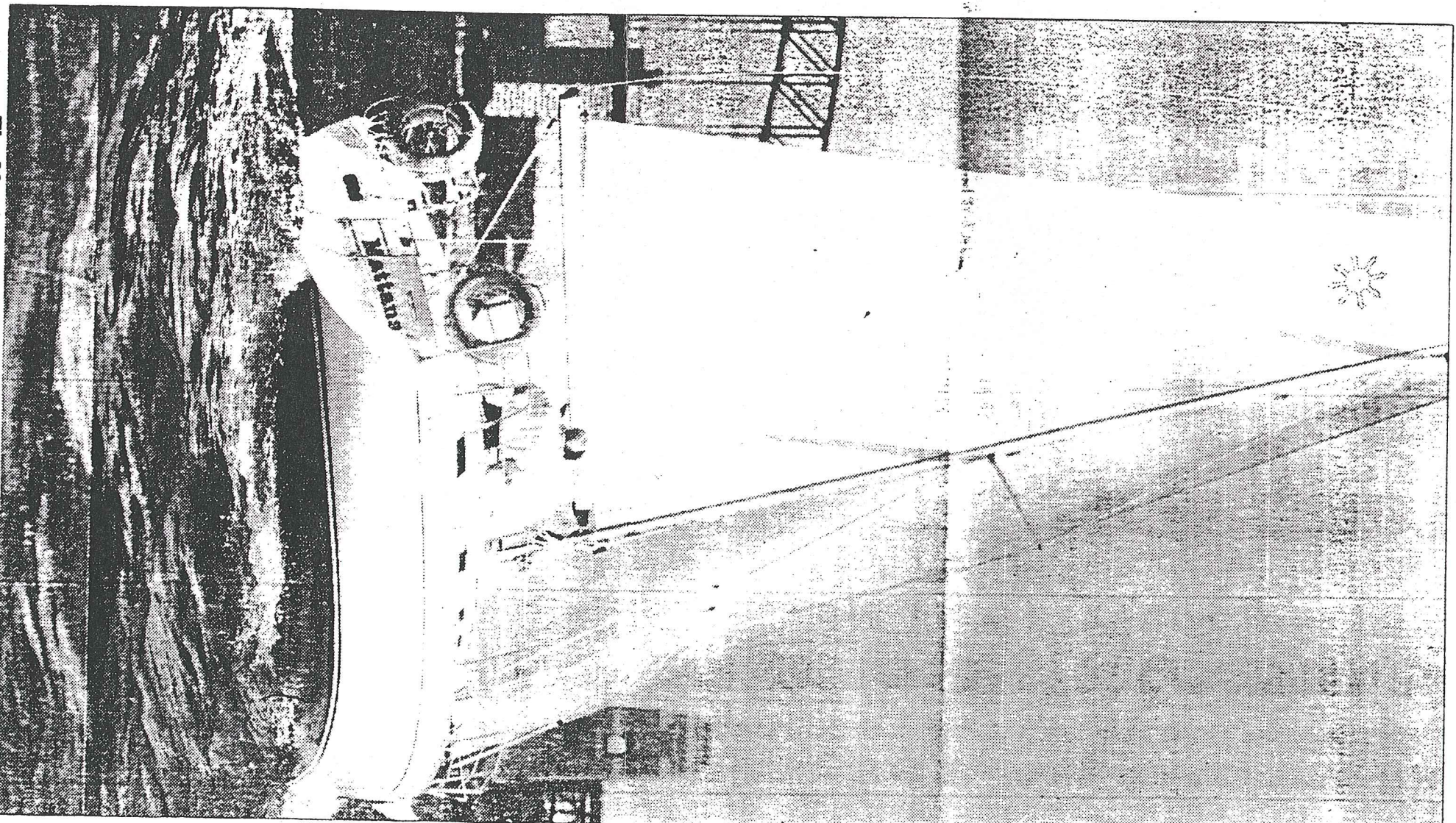
A beautifully finished teak table with a brilliant polish is a highlight of the saloon.

Aft of the L-shaped sofa, on the starboard side, is the well-planned, practical galley area, complete with a gimballed Plastrimo two-burner stove, pressurised hot and cold water, a refrigerated icebox and plenty of storage space.

An appealing feature was the inclusion of practical garbage bag holders, recessed into the side of the engine compartment housing, out of sight and out of mind, yet easily accessed.

Opposite was a well-thought-out navigation station, with a roomy console to accommodate instruments, a bookshelf, a hanging locker and chart space.

Auxiliary power for the Moody 35 was the proven Volvo Penta MD12040, a 39hp diesel which



The Moody 35 . . . an ideal set-up for the cruising couple.

gave the vessel plenty of "grunt" with its deep down torque to manoeuvre in and out of a marina berth.

Access to the engine was excellent — no problems in carrying out maintenance and general care.

Out on a fairly breezy Sydney Harbour, the Moody 35 proved how easy it is to sail.

As betis such a craft, it was fitted with a mainsail furler (Lucas) and a furling (Furlex) headsail system.

On a reach and running before the wind, the yacht had a sure feel, a big rudder and large destroyer-

style helm making for good control.

Movement through the water was, for want of a better word, slick, a strong wash showing aft from the neat, moulded transom in the traditional stepped style.

In the cockpit, controls were all within reach, and deck fittings, lifelines, spars and rigging were quality stock.

While the traditionalists of yacht racing may find fault with the Moody 35, I would have no problem (other than finding the purchase price) being the skipper of this finely built craft. With a couple of good deck-

hands, a cruise up to the Whitsundays would be a great way to go aboard a Moody 35.

SPECIFICATIONS

LOA: 10.66-metres (35ft)

Beam: 3.62-metres (11ft 8in)

Draft: 1.60-metres (5ft)

Displacement: 5,811 kg (5.8 tonnes)

Fuel capacity: 186 litres (41 gal)

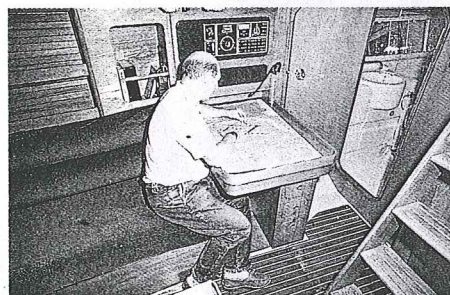
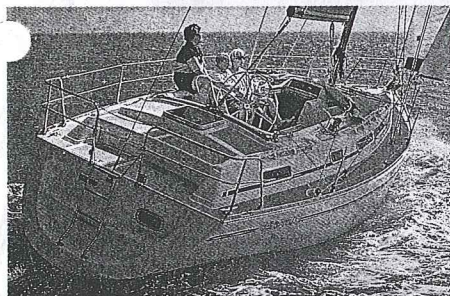
Water capacity: 195 litres (42 gal)

Ballast: 2,150 kg

Price: from \$206,000

Contact Careening Cove Boat Brokerage, Millsons Point, telephone (02) 957 3353.

ON TEST



Top, the Moody 35 with a 140 per cent headsail which is an extra. Middle, the seat in the aft cabin coachroof is clearly seen in this shot. It doubles as stowage for warps. Above, the navigator has to sit on a settee berth to work at the aft facing table

When Moody of Swanwick launched their 346 in the mid-1980s they started a highly successful run which, to date, has totalled more than 160 yachts. Development continues and trends change. Moody felt that the time had come to replace this family sailing cruiser with a more stylish and perhaps more up-to-date design – one which might include some of the observations of the more knowledgeable owners of 346s.

Comfort below was of prime importance, as was better accommodation at each end of the yacht. Many owners also

Accommodation

From the cockpit, there is only a 10in (254mm) sill to step over to land on the top tread of the gently sloped companion ladder. This is easy to negotiate and one can scuttle below facing forwards without any fear of striking one's forehead smartly on the forward edge of the hatch opening. The lowest tread is to be increased in depth – a useful feature, because you can tell without looking that the next step takes you onto the cabin sole. Stainless steel handrails are to be fitted each side of the hatchway.

At the foot of the companion, the light

MOODY 35

Good-looking, easy to handle and fast to sail, the Moody 35 is a worthy successor to that success story of the Eighties, the Moody 346

wished to see an improved galley and lighter interior. It was essential also that the yacht had a handsome outboard profile.

Currently, an impressive number of new yachts between 34ft and 37ft (10.36m – 11.28m) is being produced by major European builders, so it is understandable that, with things moving ahead, Moody have no wish to be in the market with a design which must be considered a little long in the tooth.

We were interested to see just how Moody's new yacht compared with stiff opposition and looked forward to putting her through her paces in the Solent...

and airy saloon is particularly welcoming. This cheerful ambience is partly achieved by using a very light biscuit colour for the panels lining the deckhead and the inside of the cabin trunk. Large, aluminium framed windows also help.

The impression of length is emphasised by the settees and the centre section of the two-leaved cabin table tapering in towards the centreline at the forward end. The double-rolled settee backs continue across the cabin forward bulkhead each side of the door and through to the forecabin. This does much to increase the cosiness of the living area. The 5in (127mm) thick

COMPARABLE BOATS

	MOODY 35		GIB'SEA 352		SEAHAWK		FEELING 1090	
LOA	34ft 6in	10.52m	34ft 7in	10.55m	34ft 7in	10.54m	36ft 6in	11.13m
LWL	28ft 10in	8.78m	28ft 0in	8.55m	27ft 0in	8.23m	29ft 2in	8.89m
Beam	11ft 10½in	3.62m	11ft 4in	3.45m	12ft 3in	3.74m	11ft 10in	3.61m
Draught (Fin)	5ft 3in	1.60m	5ft 7in	1.70m	4ft 11in	1.50m	5ft 10in	1.78m
Disp (Fin)	12,800lb	5,806kg	10,144lb	4,700kg	14,366lb	6,530kg	12,566lb	6,530kg
Ballast (Fin)	4,750lb	2,155kg	4,056lb	1,840kg	5,690lb	2,590kg	4,750lb	2,150kg
Sail area (inc 100 per cent foreΔ)	508.6ft ²	47.25m ²	563ft ²	52.35m ²	638ft ²	59m ²	448ft ²	41.62m ²
Berths	6		6/8		7		7/9	
Engine	Thornycroft T80		Yanmar 3GM		Volvo 2003		Volvo 2003	
Water	35hp	26.08kW	28hp	20.85kW	28hp	20.85kW	28hp	20.85kW
Fuel	40 gal	181.84lt	70.40 gal	320lt	60 gal	270lt	90 gal	409.14lt
Sail area: Disp	40 gal	181.84lt	13.20 gal	60lt	35 gal	160lt	30 gal	136.38lt
Price (ex VAT)	14.92		19.29		15.08		13.31	
	238.47		206.23		325.83		226.10	
	£52,250		£50,950		£59,990		£59,500	

ON TEST

upholstery is well made and firm; this ensures comfort when sitting below for long periods. Both settees are very comfortable as berths.

Well-matched teak is used throughout the accommodation and the basic standard of joiner work is good. Behind the settee backs, there are cupboards at each corner of the saloon with a shelved area in between on both sides. The topsides themselves are ceiled with spaced teak battening.

Handholds in the saloon are provided by the pillar supporting the deck-stepped mast and a wooden pillar down to the corner of the galley peninsula. There are no other handholds in the saloon: a finger rail running along the bottom of the trunk each side would be an improvement (and somewhere to tuck the bottom of the curtains instead of running them on the present thin teak battens attached by light, aluminium strip brackets). Grab handles should also be fitted each side of the doorway in the forward bulkhead.

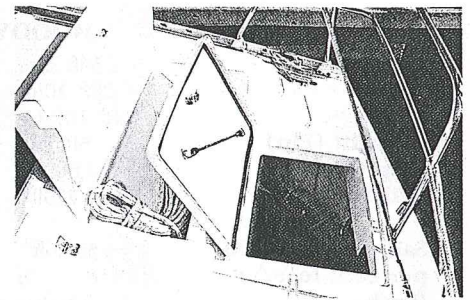
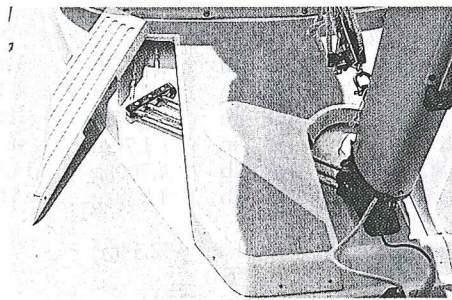
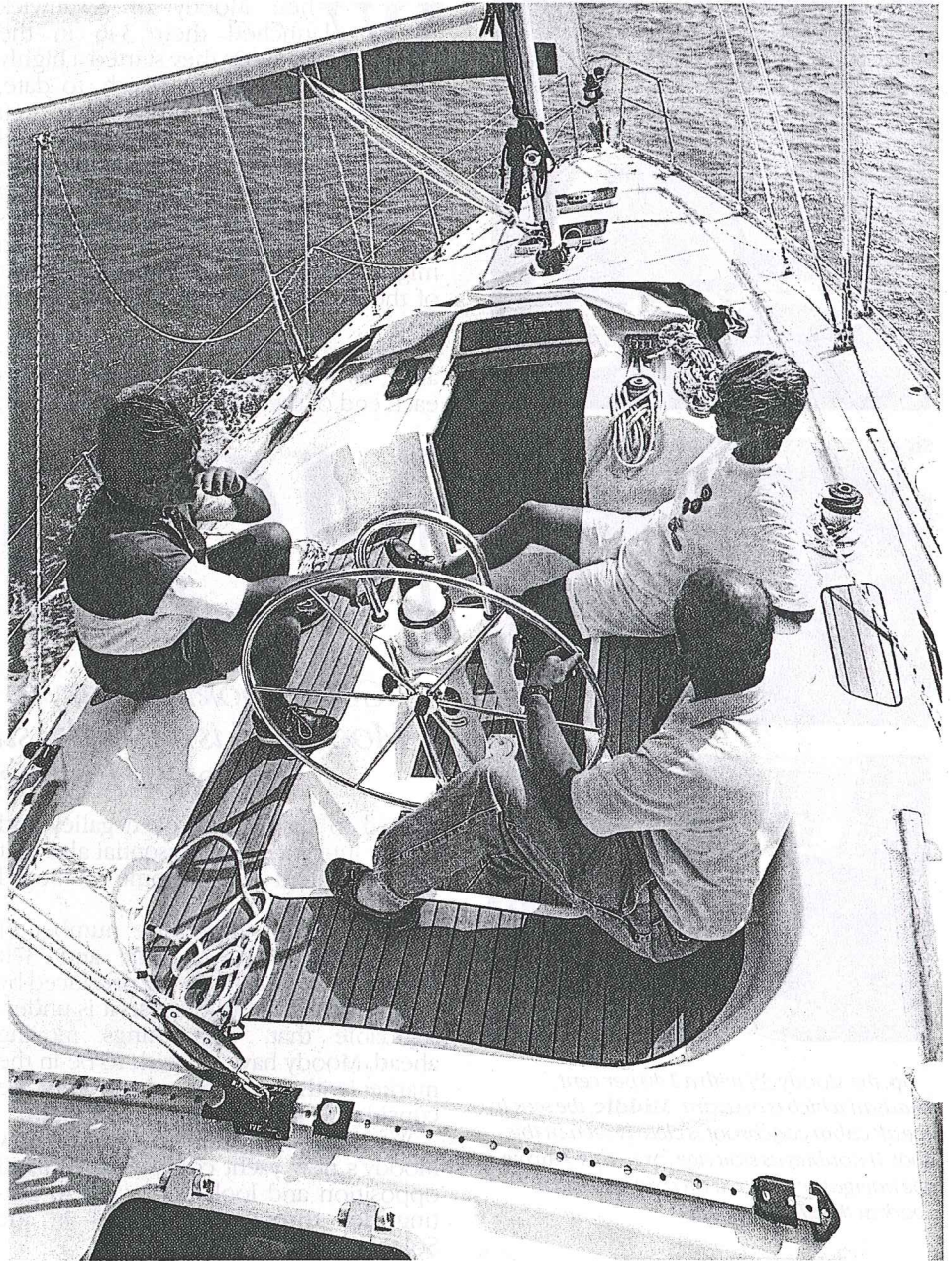
The L-shaped galley, to starboard, occupies the outboard side of the passageway through to the after cabin. There is plenty of stowage behind the worktop and, with the cover fitted over the Plastimo two-burner cooker, generous put-down area. Additional useful space adjacent to the galley is provided by the Formicaed area which extends across the top of the engine box just abaft the companion ladder.

Ventilation in this area is via the main hatch and an opening light in the cabin trunk at the after end of the galley. The two deep circular stainless steel sinks in the peninsula are served by pressure hot and cold fresh water. A large, refrigerated icebox is standard.

In the navigating area, at the after end of the saloon on the port side, there is a chart table which will take a half-folded Admiralty chart. The navigator, however, must work seated on the after end of the port settee, facing aft. At present, display area for instruments is limited, but adequate space for a yacht of this class will be available when the electrics panel and the display area beneath are reorganised.

Continuing aft from the chart area one enters the large head and shower compartment which is separated from the navigating station by a good sized wet locker, draining into the bilge. The head/shower compartment is spacious, well ventilated and easy to clean. All seacocks are readily accessible.

The head/shower compartment can be made en suite with the after cabin by passing through an Alice-in-Wonderland doorway. A soft roll finish to the head of



Top, the ergonomics of the cockpit are very good. The radiused corners make for real comfort. **Above left**, all control lines are led aft beneath these removable covers. **Above right**, this shows the lazarette hatch cover (right) and lid over the bench seat

ON TEST

upholstery is well made and firm; this ensures comfort when sitting below for long periods. Both settees are very comfortable as berths.

Well-matched teak is used throughout the accommodation and the basic standard of joiner work is good. Behind the settee backs, there are cupboards at each corner of the saloon with a shelved area in between on both sides. The topsides themselves are ceiled with spaced teak battening.

Handholds in the saloon are provided by the pillar supporting the deck-stepped mast and a wooden pillar down to the corner of the galley peninsula. There are no other handholds in the saloon: a finger rail running along the bottom of the trunk each side would be an improvement (and somewhere to tuck the bottom of the curtains instead of running them on the present thin teak battens attached by light, aluminium strip brackets). Grab handles should also be fitted each side of the doorway in the forward bulkhead.

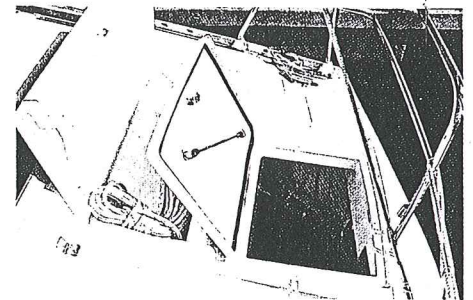
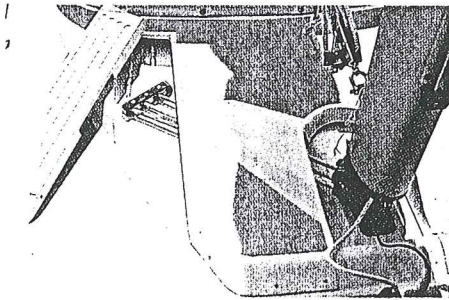
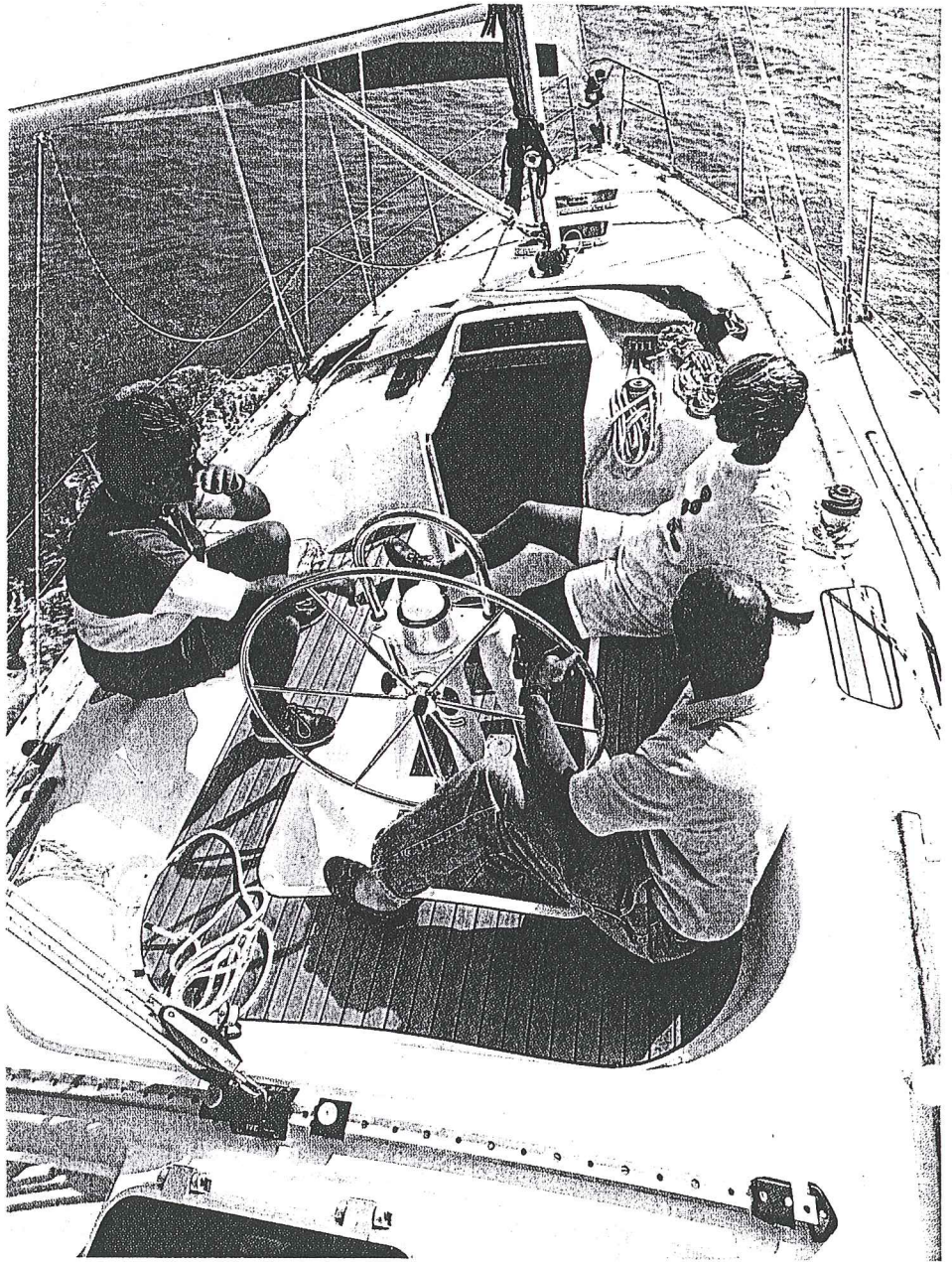
The L-shaped galley, to starboard, occupies the outboard side of the passageway through to the after cabin. There is plenty of stowage behind the worktop and, with the cover fitted over the Plastimo two-burner cooker, generous put-down area. Additional useful space adjacent to the galley is provided by the Formicaed area which extends across the top of the engine box just abaft the companion ladder.

Ventilation in this area is via the main hatch and an opening light in the cabin trunk at the after end of the galley. The two deep circular stainless steel sinks in the peninsula are served by pressure hot and cold fresh water. A large, refrigerated icebox is standard.

In the navigating area, at the after end of the saloon on the port side, there is a chart table which will take a half-folded Admiralty chart. The navigator, however, must work seated on the after end of the port settee, facing aft. At present, display area for instruments is limited, but adequate space for a yacht of this class will be available when the electrics panel and the display area beneath are reorganised.

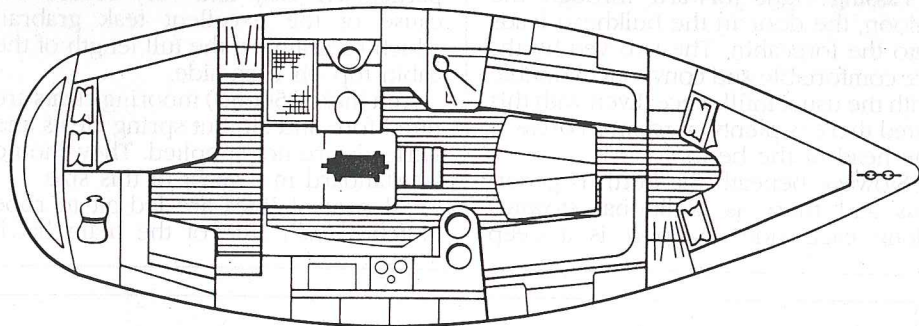
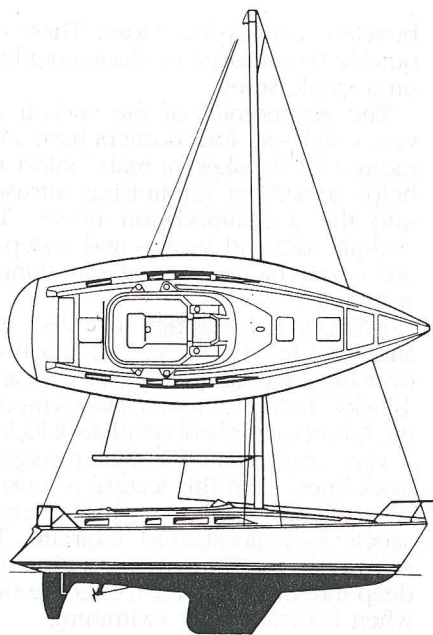
Continuing aft from the chart area one enters the large head and shower compartment which is separated from the navigating station by a good sized wet locker, draining into the bilge. The head/shower compartment is spacious, well ventilated and easy to clean. All seacocks are readily accessible.

The head/shower compartment can be made en suite with the after cabin by passing through an Alice-in-Wonderland doorway. A soft roll finish to the head of



Top, the ergonomics of the cockpit are very good. The radiused corners make for real comfort. **Above left**, all control lines are led aft beneath these removable covers. **Above right**, this shows the lazarette hatch cover (right) and lid over the bench seat

ON TEST



Designed by: Bill Dixon, Greydowns, School Road, Old Bursledon, Southampton, Hampshire SO3 8BX. Tel: Bursledon (042121) 5280. Fax: 042121 6203.

Built by: Moody Marketing and Development Ltd, Swanwick Marina, Southampton, Hampshire SO3 72L. Tel: Locks Heath (0489) 885000. Fax: (0489) 885509.

mainsail with three slab reefs and a hanked working jib. We sailed the yacht with a 140 per cent genoa set on Selden's Furlex roller reefing gear.

A simple tackle kicking strap is standard, but a solid rod kicker would be an advantage, eliminating the risk of the end of the boom descending upon unprotected heads in the cockpit. The four-part end of boom mainsheet is taken down to the IYE track and traveller running across the after coaming of the cockpit. We found that the helmsman occasionally sat on the working part of the sheet, pulling it out of the cam cleat on the lower block of the mainsheet.

Construction

Both hull and deck are fair. The deck is carefully detailed and neatly styled. All non-slip areas have clean margins and where necessary the mounting positions for deck hardware are carefully defined. Each yacht carries a Lloyd's Hull Construction Certificate.

All structural bulkheads appeared to be adequately bonded. Plumbing and wiring runs are neatly made and correctly clipped off and pipework is double clipped to skin fittings.

Access to the engine is satisfactory, but

the builder intends to increase the size of the access panels which will make things even easier. There is some room for improvement in the sound deadening of the engine compartment: this would cut down the resonance that tends to persist in the galley area.

The cast iron ballast fin is attached to the stub keel - which forms a useful sump for bilgewater - using eleven 24mm diameter stainless steel studs through 6mm-thick plate washers.

Under power and sail

The Thornycroft T80 auxiliary drives a fixed, two-bladed propeller through a reduction gearbox. The engine started eagerly from both cold and warm and ran smoothly throughout its rev range. At 2,650 rpm it drove the yacht at a hull speed which, theoretically, is about 7.5kt. At this speed there was remarkably little squat and wavemaking was slight.

Directional stability at all speeds was good and there was little sensation of propwash over the rudder. Handling within the confines of the marina was good and the yacht was totally predictable when going astern.

Under sail she proved to be fast and well balanced on all points. The feel on

the wheel is very much a matter of personal preference. We found her pleasantly light, even when the yacht was pressed in boisterous puffs. There was rather more friction in the steering than we liked, but re-routing the conduit runs of the steering wires to reduce deviation to the minimum would do much to improve this.

Specification and conclusion

This is a yacht which is built and fitted out to a good standard. In standard trim it is offered with pressure hot and cold freshwater, a good sized refrigerator and very comfortable upholstery. Depth sounder and log are fitted within the basic price. This also includes the sprayhood, ground tackle, warps and fenders, two dry powder fire extinguishers, a gas bottle, first aid kit and tool kit.

Family sailors will find the Moody 35 most attractive. Good looks go hand in hand with a very satisfactory performance and real ease of handling. She is roomy and comfortable and, at the after end, one is sure to get used to those Alice-in-Wonderland doors...

A very worthy successor to the 346. ■

ON TEST

the opening would reduce the force of inevitable cranial contact. . .

Main access to the after cabin is at the after end of the galley. The door here is also, of necessity, very low. Once inside, however, the after cabin is light and airy and there is generous standing headroom at the forward end of the double berth. The headboard is likely to be used as a hand hold when making one's way to the head. To withstand normal use at sea, this needs to be more substantial.

On the port side of the cabin is a comfortable settee and vanity unit with mirror above. This is to be increased in area in subsequent yachts. Stowage is generous. A nice touch is the opening port in the transom at the foot of the berth which admits light to an otherwise dark corner.

Passing right forward through the saloon, the door in the bulkhead leads into the forecabin. The two vee berths are comfortable and convert to a double with the usual infill piece. Even with this fitted there is plenty of room to dress at the head of the berth.

Stowage beneath the berth is generous and there is additional stowage along each side. To port is a deep

hanging locker with a shelved locker and vanity opposite.

On deck

Neatly cut-in areas of effective non-slip are gelled into all walk-on surfaces on the upper deck. They are finished in a light grey which relieves the basic white of the moulding.

A 35lb (16kg) Harbourfast plough anchor is supplied as standard. This stows either over a stemhead roller or in the anchor well which contains a plinth to take an anchor windlass (not standard). The chain is led via a navel pipe to a cable locker: the chain requires some encouragement to run below. Although the shrouds land in the middle of the side decks, passage fore and aft is particularly easy and very secure because of the excellent teak grabrail which runs almost the full length of the cabin top on each side.

Ten inch (250mm) mooring cleats are fitted fore and aft, but spring cleats and fairleads are not supplied. They should be standard in a yacht of this size.

All control lines are led aft to rope clutches each side of the main hatch

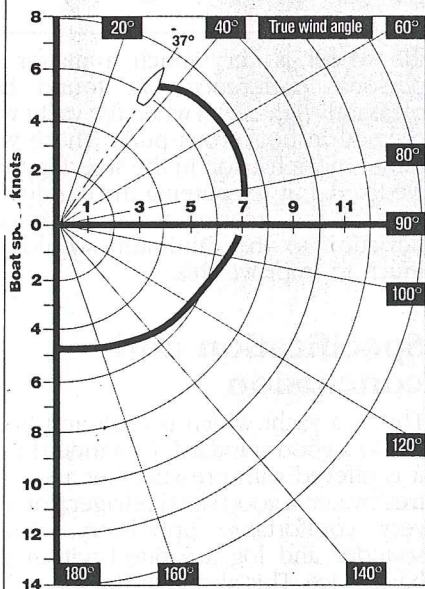
beneath neatly styled covers. These can quickly be removed by slackening back on a single screw.

The ergonomics of the cockpit are very good – all four corners have a full radius which makes for real comfort and helps greatly in minimising intrusion into the accommodation below. The cockpit sole and seats – and a step-on area on top of the coaming – are skinned with laid teak.

Bill Dixon has achieved a very neat after end for the yacht. The coachroof over the after cabin does not look at all 'blocky'. It ends in a neat seat formed by the lids of a couple of small deck lockers – very convenient for the stowage of dock lines. Aft this feature is a useful area of after deck, in which there is access to a good-sized lazarette. The retroussée transom has been given two deep indents which form effective steps when boarding after swimming.

Rig and sails

The three-panel, deck-stepped Kemp mast is set fairly well aft in the yacht giving a large foretriangle. Standard sails supplied within the basic price are a



Test conditions

Wind speed: 13/14 knots
Sea state: slight
Sail combination: Mainsail and 150 per cent furling genoa

Engine trials – decibel levels/speed

Speed	3kt	4kt	5kt	6kt	7kt	7.6kt
Forecabin	61	65	64	70	70	72
Saloon	69	73	73	72	78	79
Charts	72	78	73	76	81	81
Head	72	77	78	79	84	86
Galley	73	82	77	77	82	86
After cockpit	75	73	75	77	82	88
Cockpit	68	73	70	76	78	81

Speed	rpm	Speed	rpm
3.8kt	1,000	7.3kt	2,500
5.4kt	1,500	7.6kt	2,800
6.6kt	2,000		

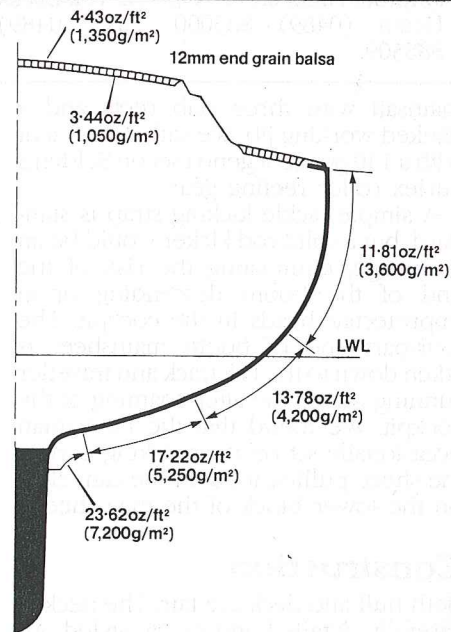
Stowage factor: 3ft³ per person, for stowage of personal effects. Factor is shown as a percentage of total volume. **Pounds/inch (kg/cm) immersion:** How much weight it takes to sink the boat parallel to DWL. **Prismatic coefficient:** The ratio of volume displacement to a volume at LWL and the max cross sectional area below the load waterplane. An indication of the fineness or fullness of the hull. **Polar diagram:** Shows the optimum closehauled angle to the true wind. Also shows speed attained on all courses. **Important – Consider in conjunction with the true wind speed during the**

test. Sail area displacement ratio: This ratio gives some indication of power available. Higher numbers = greater performance.

$$\frac{SA(ft^2)}{(\text{Displacement (lb)} \div 64)^{0.66}}$$

Ballast ratio: A comparison between displacement and the weight of ballast. **Displacement waterline length:** performance indicator. Low Nos. = higher performance.

$$\frac{(\text{Displacement (lb)} \div 2240)}{(0.01 \times \text{LWL (ft)})^3}$$



Factors

Prismatic coefficient 0.554
Immersion 1,035lb/in (407.48kg/cm)
Ballast ratio 37.11 per cent
Personal stowage 17.21 per cent

Stowage volumes

Forecabin	31.49ft ³	0.89m ³
Saloon	18.15ft ³	0.51m ³
Charts	1.38ft ³	0.04m ³
Galley	23.07ft ³	0.65m ³
Head	4.53ft ³	0.13m ³
After cabin	25.95ft ³	0.74m ³
Total	104.57ft ³	2.96m ³

ON TEST

the opening would reduce the force of inevitable cranial contact. . .

Main access to the after cabin is at the after end of the galley. The door here is also, of necessity, very low. Once inside, however, the after cabin is light and airy and there is generous standing headroom at the forward end of the double berth. The headboard is likely to be used as a hand hold when making one's way to the head. To withstand normal use at sea, this needs to be more substantial.

On the port side of the cabin is a comfortable settee and vanity unit with mirror above. This is to be increased in area in subsequent yachts. Stowage is generous. A nice touch is the opening port in the transom at the foot of the berth which admits light to an otherwise dark corner.

Passing right forward through the saloon, the door in the bulkhead leads into the forecabin. The two vee berths are comfortable and convert to a double with the usual infill piece. Even with this fitted there is plenty of room to dress at the head of the berth.

Stowage beneath the berth is generous and there is additional stowage along each side. To port is a deep

hanging locker with a shelved locker and vanity opposite.

On deck

Neatly cut-in areas of effective non-slip are gelled into all walk-on surfaces on the upper deck. They are finished in a light grey which relieves the basic white of the moulding.

A 35lb (16kg) Harbourfast plough anchor is supplied as standard. This stows either over a stemhead roller or in the anchor well which contains a plinth to take an anchor windlass (not standard). The chain is led via a navel pipe to a cable locker: the chain requires some encouragement to run below. Although the shrouds land in the middle of the side decks, passage fore and aft is particularly easy and very secure because of the excellent teak grabrail which runs almost the full length of the cabin top on each side.

Ten inch (250mm) mooring cleats are fitted fore and aft, but spring cleats and fairleads are not supplied. They should be standard in a yacht of this size.

All control lines are led aft to rope clutches each side of the main hatch

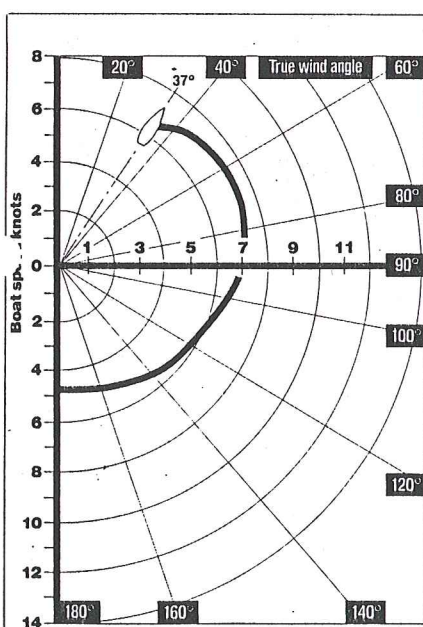
beneath neatly styled covers. These can quickly be removed by slackening back on a single screw.

The ergonomics of the cockpit are very good – all four corners have a full radius which makes for real comfort and helps greatly in minimising intrusion into the accommodation below. The cockpit sole and seats – and a step-on area on top of the coaming – are skinned with laid teak.

Bill Dixon has achieved a very neat after end for the yacht. The coachroof over the after cabin does not look at all 'blocky'. It ends in a neat seat formed by the lids of a couple of small deck lockers – very convenient for the stowage of dock lines. Aft this feature is a useful area of after deck, in which there is access to a good-sized lazarette. The retroussée transom has been given two deep indents which form effective steps when boarding after swimming.

Rig and sails

The three-panel, deck-stepped Kemp mast is set fairly well aft in the yacht giving a large foretriangle. Standard sails supplied within the basic price are a



Test conditions

Wind speed: 13/14 knots
Sea state: slight
Sail combination: Mainsail and 150 per cent furling genoa

Engine trials – decibel levels/speed

Speed	3kt	4kt	5kt	6kt	7kt	7.6kt
Forecabin	61	65	64	70	70	72
Saloon	69	73	73	72	78	79
Charts	72	78	73	76	81	81
Head	72	77	78	79	84	86
Galley	73	82	77	77	82	86
After cockpit	75	73	75	77	82	88
Cockpit	68	73	70	76	78	81

Speed	rpm	Speed	rpm
3.8kt	1,000	7.3kt	2,500
5.4kt	1,500	7.6kt	2,800
6.6kt	2,000		

Stowage factor: 3ft³ per person, for stowage of personal effects. Factor is shown as a percentage of total volume. **Pounds/inch (kg/cm) immersion:** How much weight it takes to sink the boat parallel to DWL. **Prismatic coefficient:** The ratio of volume displacement to a volume at LWL and the max cross sectional area below the load waterplane. An indication of the fineness or fullness of the hull. **Polar diagram:** Shows the optimum closehailed angle to the true wind. Also shows speed attained on all courses. **Important – Consider in conjunction with the true wind speed during the**

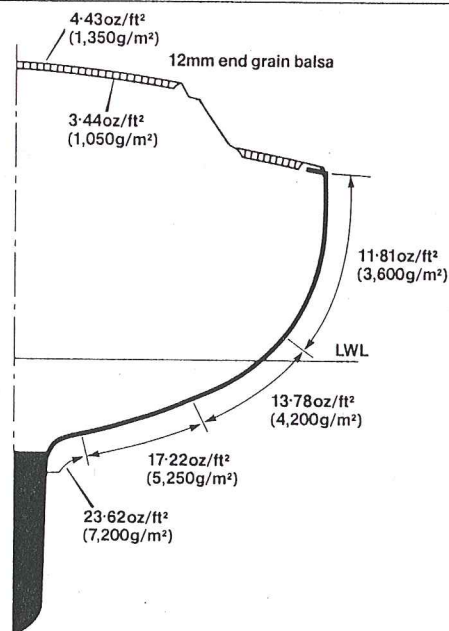
test. Sail area displacement ratio: This ratio gives some indication of power available. Higher numbers = greater performance.

$$SA(ft^2)$$

$$\frac{\text{Displacement (lb)} \div 64}{100}$$

Ballast ratio: A comparison between displacement and the weight of ballast. **Displacement waterline length:** performance indicator. Low Nos. = higher performance.

$$\frac{\text{Displacement (lb)} \div 2240}{(0.01 \times LWL (ft))^3}$$



Factors

Prismatic coefficient 0.554
Immersion 1,035lb/in (407.48kg/cm)
Ballast ratio 37.11 per cent
Personal stowage 17.21 per cent

Stowage volumes

Forecabin	31.49ft ³	0.89m ³
Saloon	18.15ft ³	0.51m ³
Charts	1.38ft ³	0.04m ³
Galley	23.07ft ³	0.65m ³
Head	4.53ft ³	0.13m ³
After cabin	25.95ft ³	0.74m ³
Total	104.57ft ³	2.96m ³